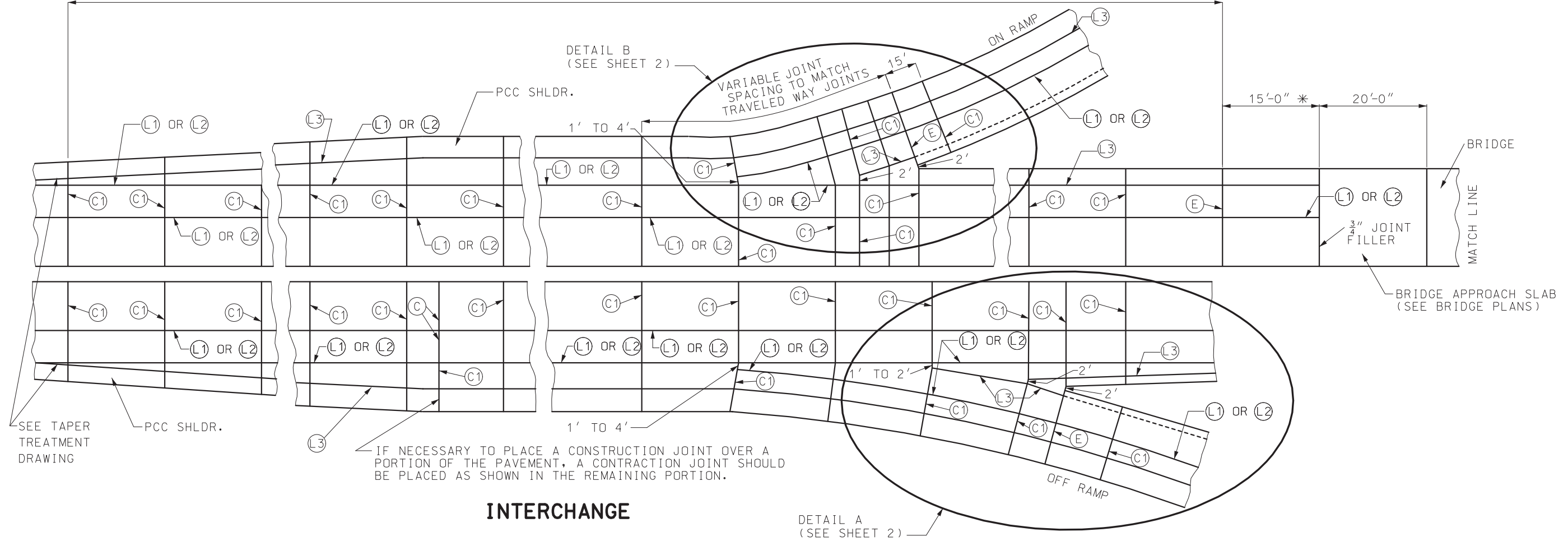
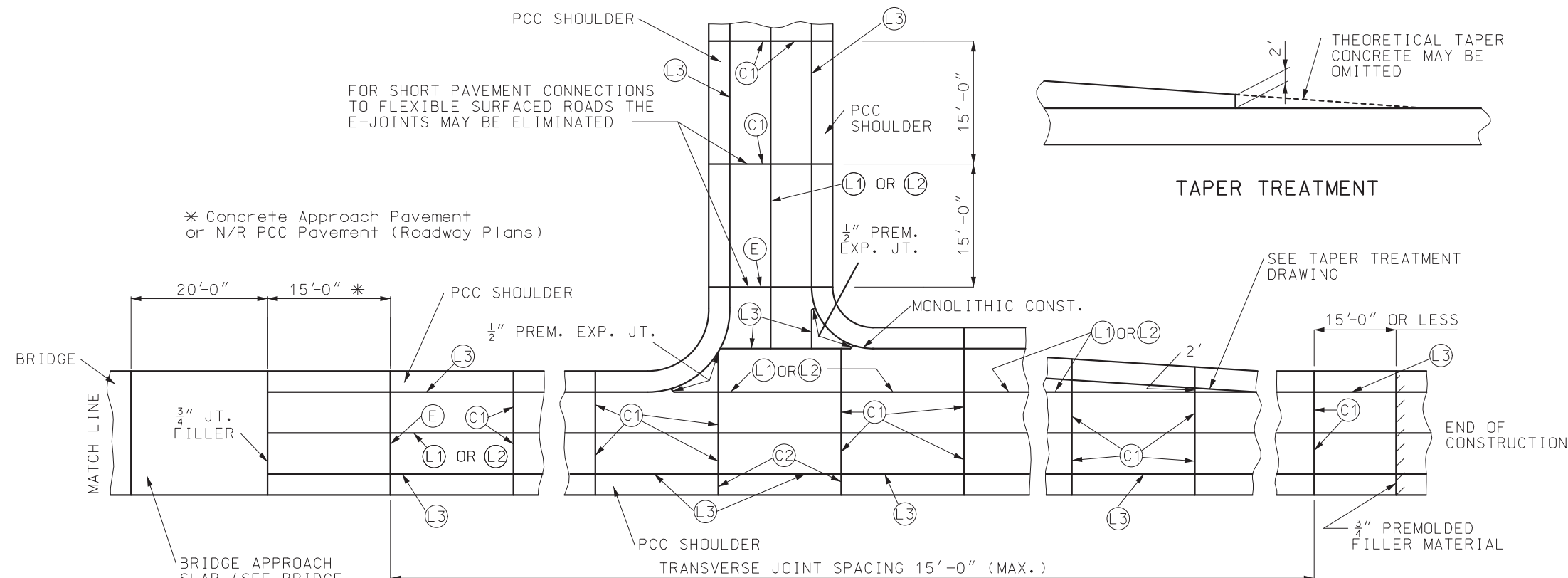


TRANSVERSE JOINT SPACING 15'-0" (MAX.)



INTERCHANGE

DETAIL A (SEE SHEET 2)



NON-INTERCHANGE

GENERAL NOTES:

ALL TRANSVERSE JOINTS ON SHOULDERS SHALL BE C2 .
THE CONTRACTOR SHALL DETERMINE THE PAVING WIDTH.

THE INTERCHANGE WILL EXTEND FROM THE BEGINNING OF THE ACCELERATION/DECELERATION LANE TO THE GORE RETURN TAPER ON THE MAINLINE. THE INTERCHANGE WILL ALSO INCLUDE THE RAMPS UP TO THE BEGINNING OF THE RADIUS WITH THE ROAD INTERSECTING THE RAMP.

FOR JOINT DETAILS, SEE SHEETS NO. 3 & 4.

THE JOINT LAYOUT OF RAMPS IS TYPICAL FOR OUTER RAMPS OF CLOVERLEAF AND DIAMOND INTERCHANGES. SEE OTHER DRAWINGS FOR SPECIAL JOINT LAYOUTS.

JOINT SPACING SHOWN IS MAXIMUM AND IS TO BE REDUCED TO AVOID CONFLICT WITH ABUTTING STRUCTURES. JOINTS IN MULTI-LANE PAVEMENT ARE TO BE CONTINUOUS.

MoDOT MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

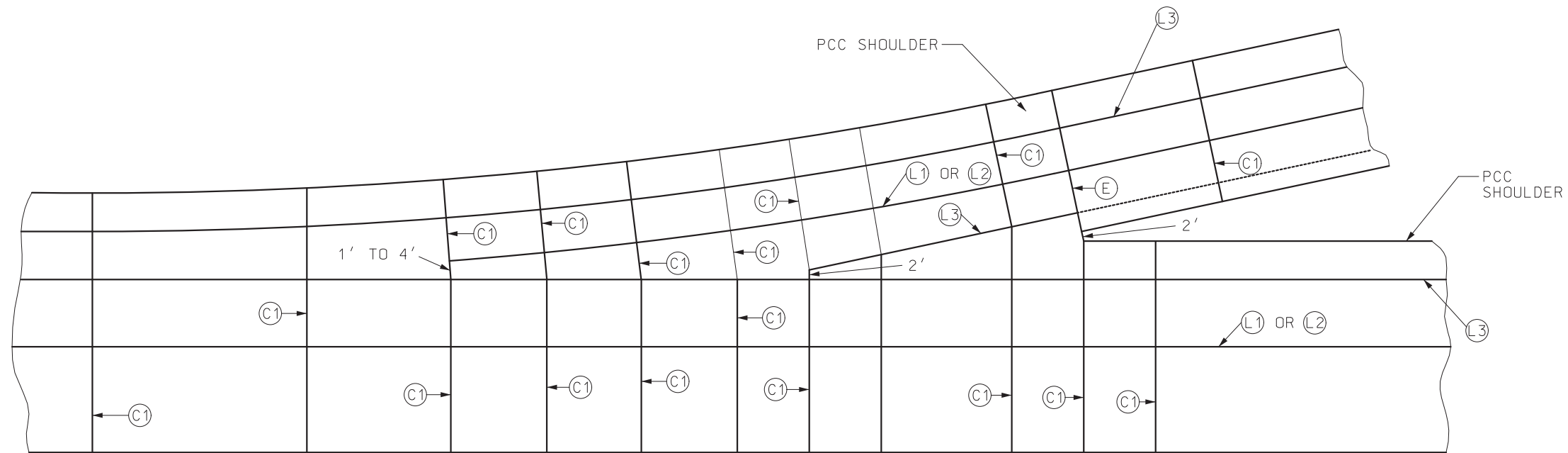
STATE OF MISSOURI
TRAVIS D. KOESTNER
NUMBER PE-30042
PROFESSIONAL ENGINEER
THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

CONCRETE PAVEMENT AND BASE APPURTENANCES FOR 15' JOINT SPACING

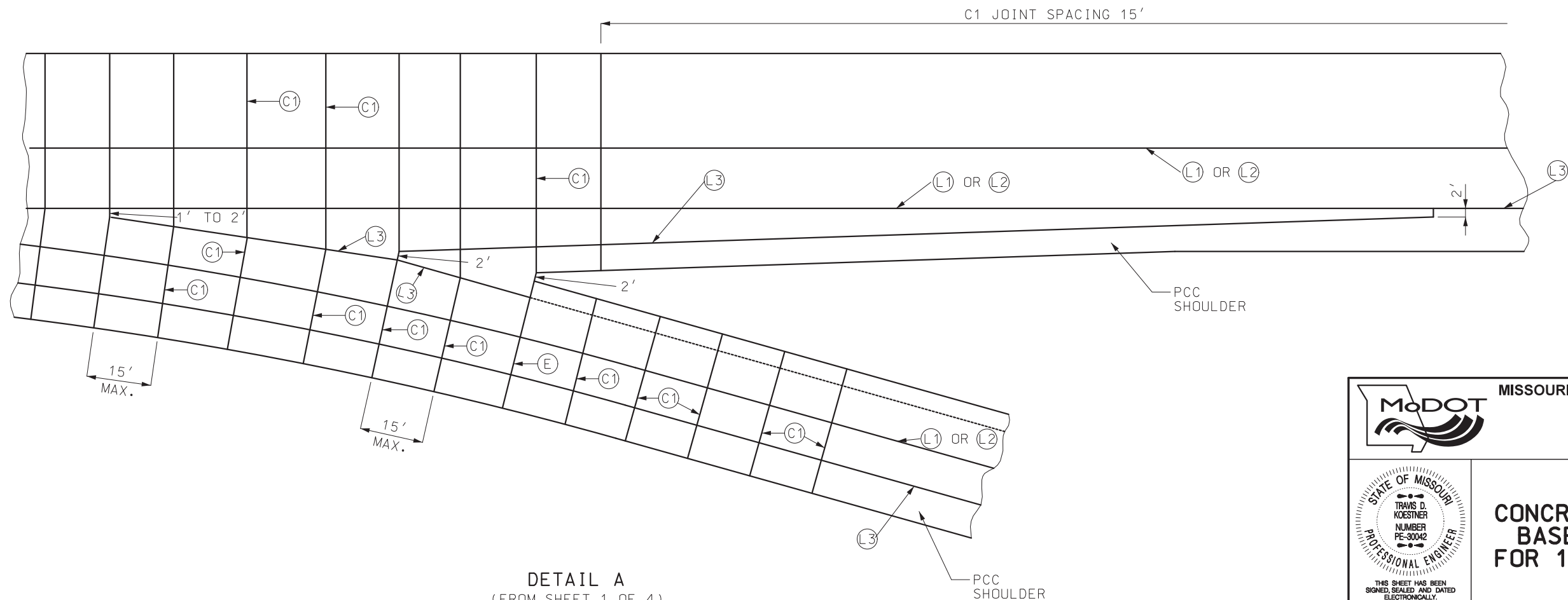
DATE EFFECTIVE: 4/1/2023	502.05S	SHEET NO. 1 OF 4
DATE PREPARED: 1/6/2023		

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.


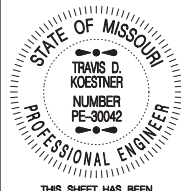
IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

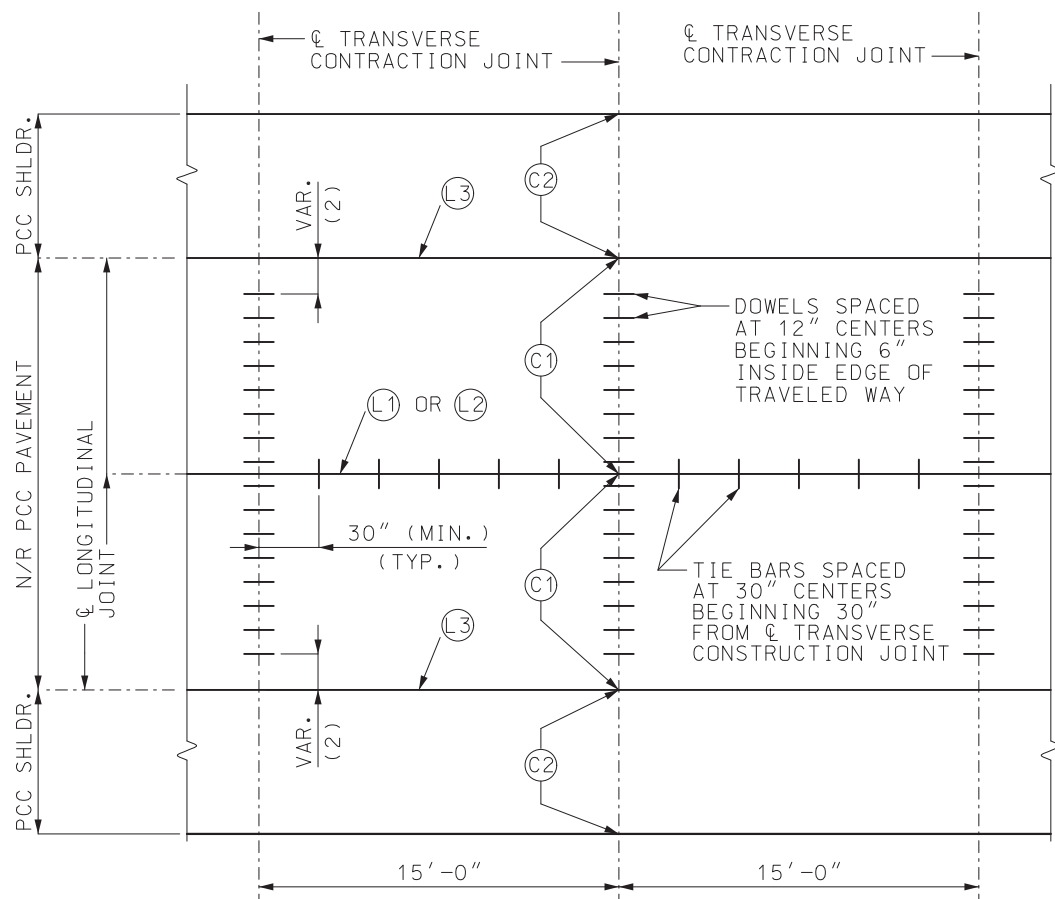


DETAIL B
(FROM SHEET 1 OF 4)



DETAIL A
(FROM SHEET 1 OF 4)

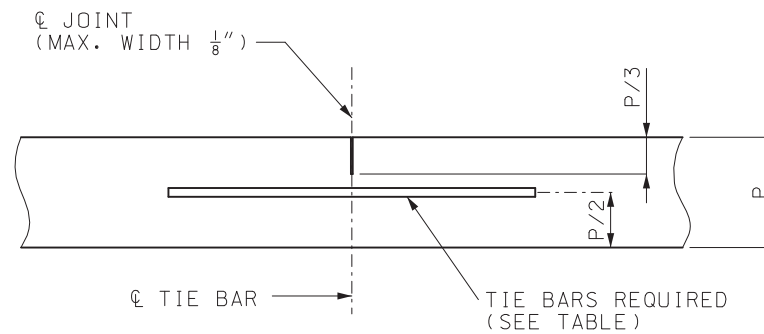
	MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION 105 WEST CAPITOL JEFFERSON CITY, MO 65102 1-888-ASK-MODOT (1-888-275-6636)	
		CONCRETE PAVEMENT AND BASE APPURTENANCES FOR 15' JOINT SPACING
DATE EFFECTIVE: 4/1/2023 DATE PREPARED: 1/6/2023	502.05S	SHEET NO. 2 OF 4



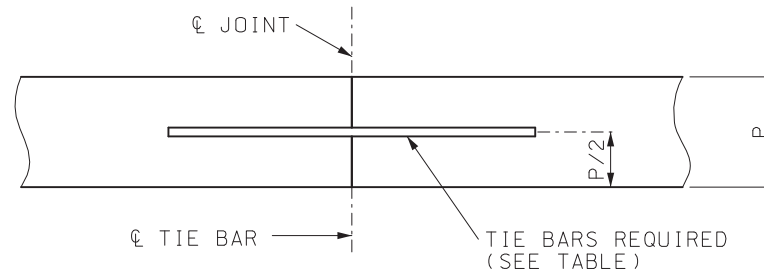
JOINT PLAN AND SPACING FOR CONTRACTION JOINTS (1)

- (1) LONGITUDINAL JOINT NOT REQUIRED FOR 4' OR NARROWER SHOULDER, PAVED MONOLITHICALLY WITH ADJACENT LANE AT THE SAME THICKNESS; FULL DEPTH SHOULDER WIDER THAN 4' SHALL REQUIRE (L1) OR (L2) JOINT INSTEAD OF (L3) JOINT.
- (2) DOWEL BARS BEGIN 6" INSIDE EDGE OF TRAVELED WAY.

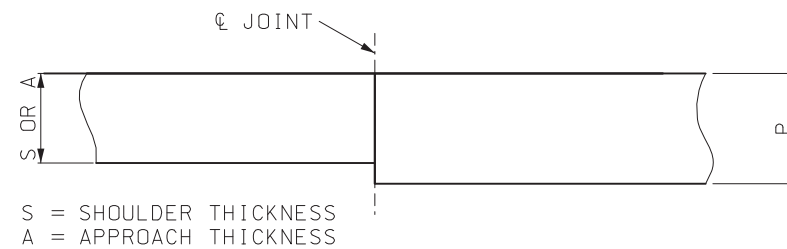
PCCP THICKNESS (P)	DOWEL		TIE BAR	
	SIZE	SPACING	SIZE	SPACING
LESS THAN 7"	NONE	NONE	#5X30"	30" CTRS.
7" TO 10"	1 1/4"X18"	12" CTRS.	#5X30"	30" CTRS.
GREATER THAN 10"	1 1/2"X18"	12" CTRS.	#6X40"	30" CTRS.



LONGITUDINAL JOINT (L1)

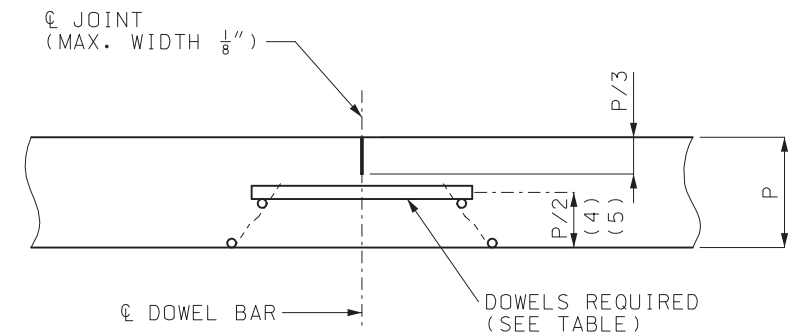


LONGITUDINAL CONSTRUCTION JOINT (L2)



LONGITUDINAL CONSTRUCTION JOINT FOR SHOULDER AND APPROACHES (L3)

S = SHOULDER THICKNESS
A = APPROACH THICKNESS

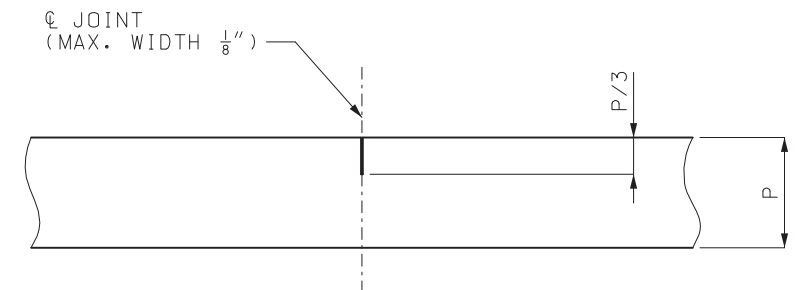


FOR PERMISSIBLE TYPES OF DOWEL SUPPORTING UNITS, SEE OTHER DRAWINGS.

TRANSVERSE CONTRACTION JOINTS FOR CONCRETE PAVEMENT OR BASE WIDENING SHALL MATCH EXISTING JOINTS.

TRANSVERSE CONTRACTION JOINT (C1) (3)

- (3) DOWEL BARS ARE REQUIRED FOR ALL PAVEMENTS HAVING THE SAME THICKNESS AS THE TRAVELED WAY.
- (4) FOR PAVEMENTS HAVING THICKNESS IN 1/2" INCREMENTS, DOWEL BASKETS SHALL BE (P-1/2")/2.
- (5) DOWEL BARS MAY BE PLACED BY MECHANICAL MEANS AT THE OPTION OF THE CONTRACTOR.



TRANSVERSE CONTRACTION JOINT (C2)

GENERAL NOTES:

THE FINAL POSITION OF ALL DOWELS AND TIE BARS SHALL BE PERPENDICULAR TO THE PLANE OF THE JOINT AND PARALLEL TO THE SURFACE OF THE PAVEMENT AND PARALLEL TO EACH OTHER.

MoDOT MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

STATE OF MISSOURI
TRAVIS D. KOESTNER
NUMBER PE-30042
PROFESSIONAL ENGINEER
THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

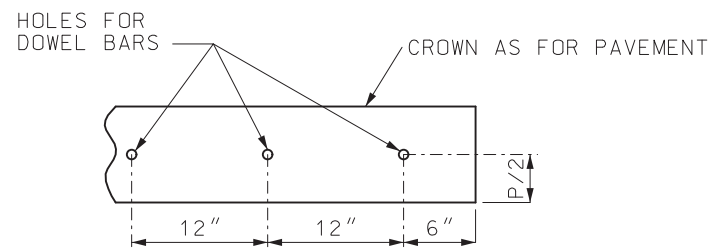
CONCRETE PAVEMENT AND BASE APPURTENANCES FOR 15' JOINT SPACING

DATE EFFECTIVE: 4/1/2023
DATE PREPARED: 1/6/2023

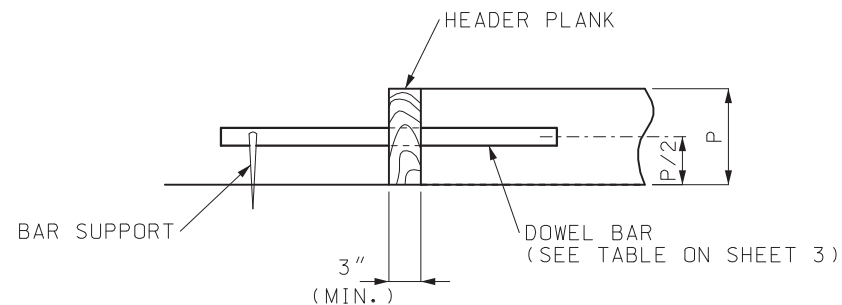
502.05S

SHEET NO.
3 OF 4

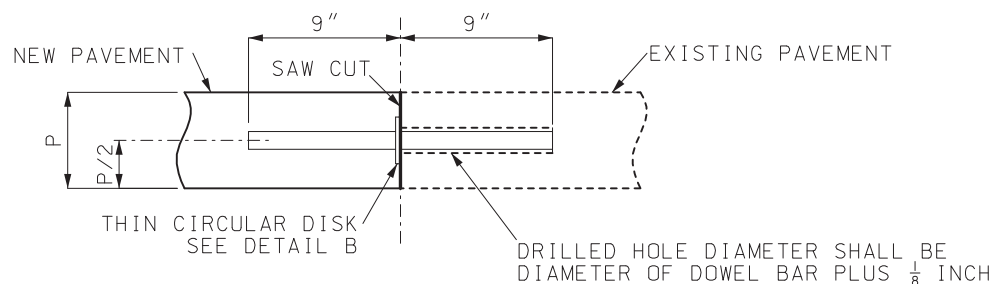
IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.



PART ELEVATION OF
HEADER PLANK



HEADER SECTION



SAWED SECTION

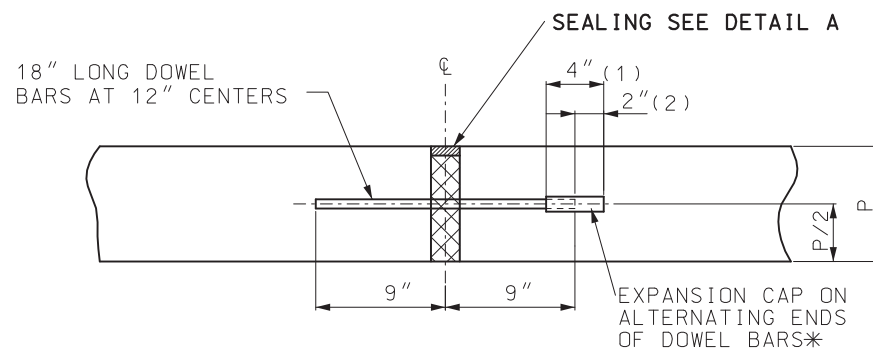
THE HEADER BOARD SHALL BE SUFFICIENTLY RIGID TO PREVENT DISTORTION FROM THE TYPICAL SECTION AND MAINTAIN A STRAIGHT LINE FROM PAVEMENT EDGE TO PAVEMENT EDGE.

THE CONSTRUCTION JOINT MAY BE SAWED FULL DEPTH. HOLES FOR DOWEL BARS SHALL BE DRILLED AFTER THE CONCRETE HAS SUFFICIENT SET TO PREVENT DAMAGE. DOWEL BARS SHALL BE BONDED INTO THE HOLES.

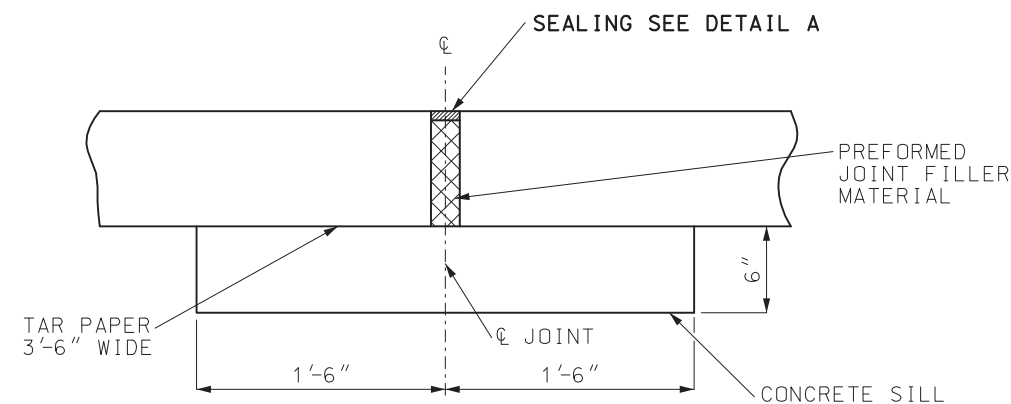
BONDING FOR DOWEL BARS SHALL BE EPOXY OR POLYESTER BONDING AGENTS AS SPECIFIED IN SECTION 1039.

THE PORTION OF THE DOWEL OUTSIDE THE HOLE SHALL BE COATED WITH AN APPROVED LUBRICANT.

CONSTRUCTION JOINT (C)

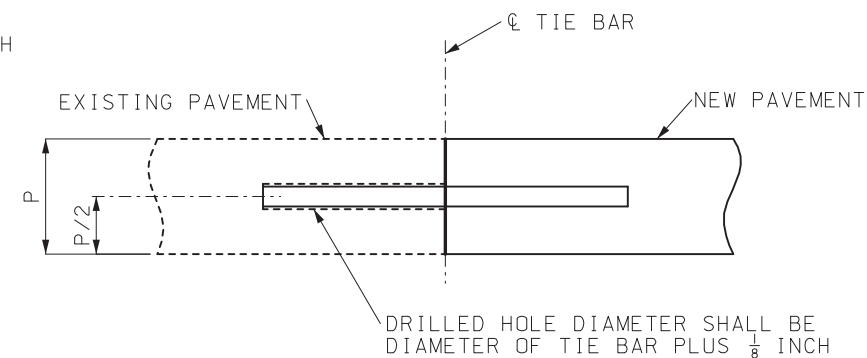


EXPANSION JOINT (E)



SILL SHALL EXTEND 18" BEYOND EACH EDGE OF THE PAVEMENT AND SHALL BE CONSTRUCTED OF CONCRETE REGARDLESS OF ADJACENT BASE MATERIAL.

ALTERNATE EXPANSION JOINT (E)
(CONTRACTOR MAY SELECT EITHER EXPANSION JOINT (E))



LONGITUDINAL CONSTRUCTION JOINT (L)
(EXISTING PAVEMENT)

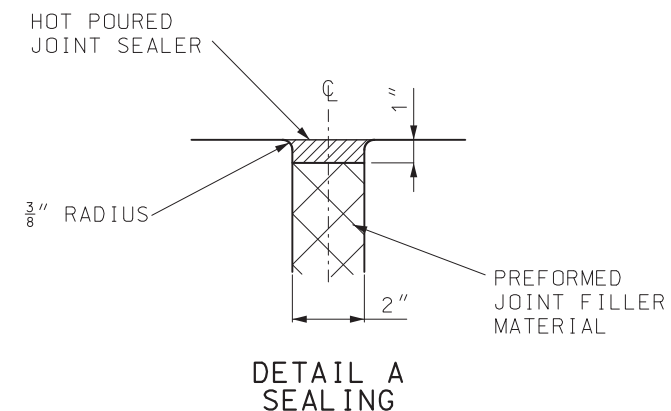
TIE BARS SHALL BE EPOXY COATED, DEFORMED REINFORCING BARS MEETING THE REQUIREMENTS OF SECTIONS 710 AND 1057.

BONDING FOR TIE BARS SHALL BE EPOXY OR POLYESTER BONDING AGENTS AS SPECIFIED IN SECTION 1039.

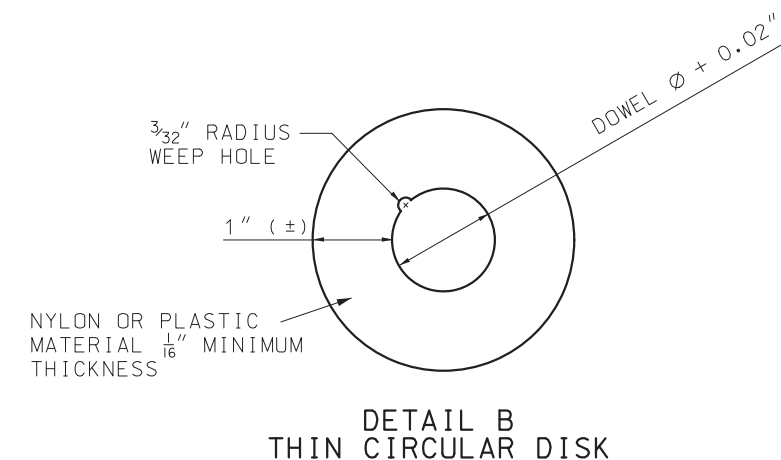
TIE BAR SIZE AND LENGTH SHALL BE BASED ON THE THICKNESS OF THE THINNER PAVEMENT OR SHOULDER TO BE TIED TOGETHER.

- (1) LENGTH OF CAP
- (2) GAP BETWEEN END OF CAP AND DOWEL.

* FOR EXPANSION JOINTS FORMED USING A CONSTRUCTION HEADER, THE EXPANSION CAPS SHALL BE INSTALLED ON THE EXPOSED END OF EACH BAR ONCE THE HEADER HAS BEEN REMOVED AND THE JOINT FILLER MATERIAL HAS BEEN INSTALLED.



DETAIL A
SEALING



DETAIL B
THIN CIRCULAR DISK

MoDOT MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

STATE OF MISSOURI
TRAVIS D. KOESTNER
NUMBER PE-30042
PROFESSIONAL ENGINEER
THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

CONCRETE PAVEMENT AND BASE APPURTENANCES FOR 15' JOINT SPACING

DATE EFFECTIVE: 4/1/2023
DATE PREPARED: 1/6/2023

502.05S

SHEET NO.
4 OF 4

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.