

PLAN

GENERAL NOTES:

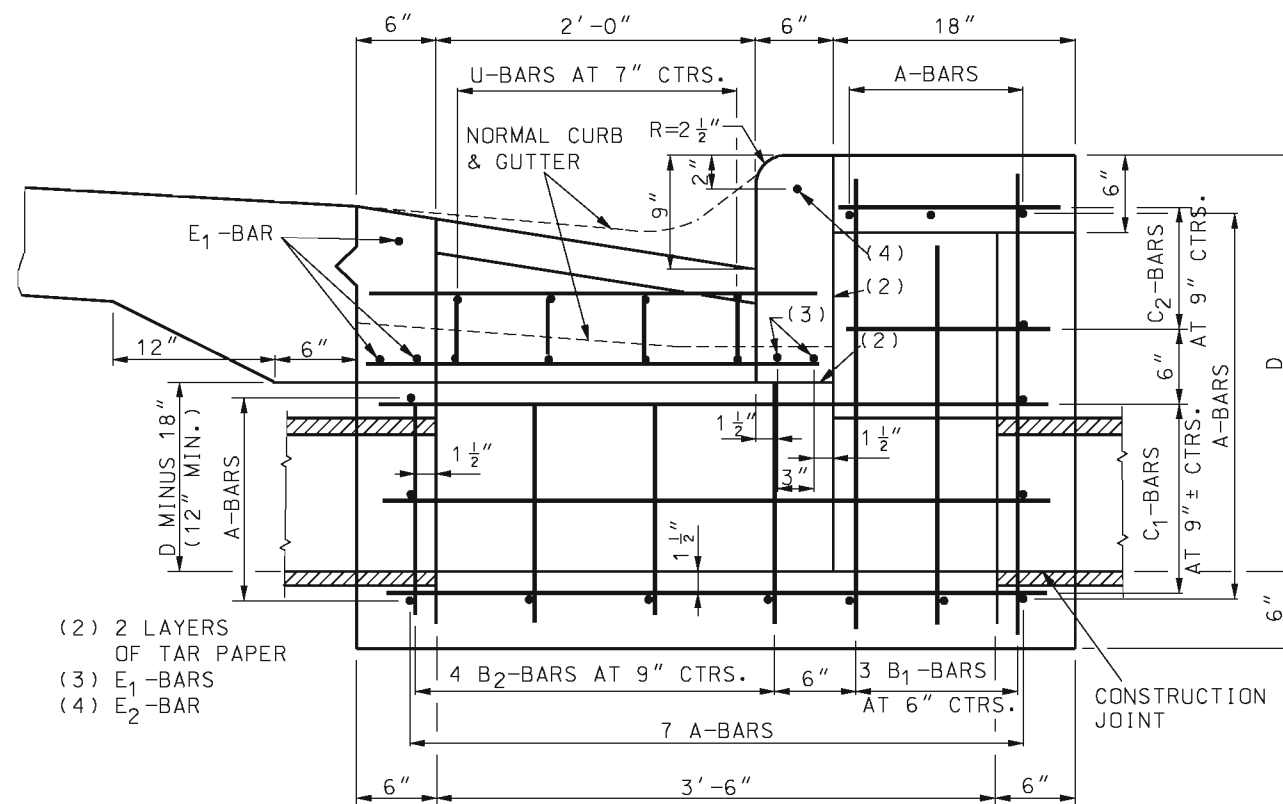
TOP OF DROP INLET WALLS SHALL BE LEVEL AND TO THE ELEVATION OF BOTTOM OF SLAB AT EDGE OF TRAVELED WAY OR BOTTOM OF CURB AND GUTTER AT DROP INLET.

ALL CONCRETE ABOVE THE TAR-PAPER SEPARATION JOINT IS TO BE CONSTRUCTED DURING PAVING OPERATIONS OR CURB AND GUTTER CONSTRUCTION; AND WILL BE PAID FOR AS SQUARE YARDS OF CONCRETE PAVEMENT OR LINEAR FEET OF CURB AND GUTTER.

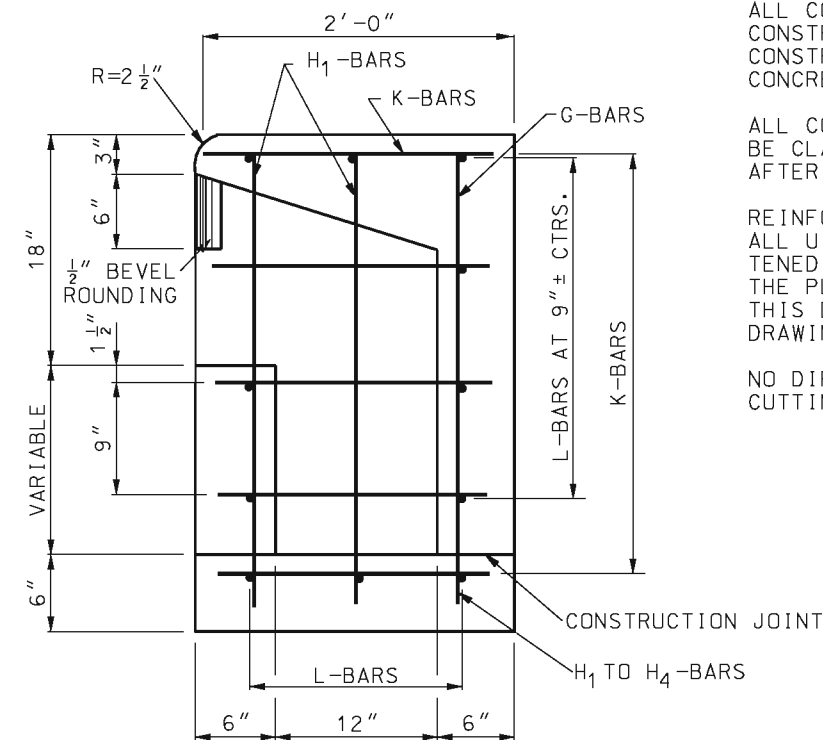
ALL CONCRETE BELOW THE TAR-PAPER SEPARATION JOINTS SHALL BE CLASS "B" CONCRETE. CONCRETE IN INVERTS SHALL BE PLACED AFTER DROP INLET HAS BEEN CONSTRUCTED.

REINFORCING BARS SHALL BE CUT AND/OR BENT AT PIPE OPENINGS. ALL U AND F-BARS SHALL BE SECURELY TIED TOGETHER AND FASTENED TO SECURE AGAINST ANY POSSIBLE DISPLACEMENT DURING THE PLACING OF CONCRETE. THE REINFORCING STEEL SHOWN ON THIS DRAWING IS IN ADDITION TO ANY REINFORCING SHOWN ON DRAWINGS FOR CONCRETE PAVEMENT OR CURB AND GUTTER.

NO DIRECT PAYMENT WILL BE MADE FOR CUTTING PIPE NOR FOR CUTTING AND BENDING REINFORCING BARS.



SECTION A-A
(INTAKE BOX)



SECTION C-C
(EXTENSION)

- (2) 2 LAYERS OF TAR PAPER
- (3) E₁-BARS
- (4) E₂-BAR

| | |
|---|------------------------------|
| MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION 105 WEST CAPITOL JEFFERSON CITY, MO 65102 1-888-ASK-MODOT (1-888-275-6636) | |
| KATHRYN PHILLIPS HARVEY NUMBER PE-23751 PROFESSIONAL ENGINEER | DROP INLET TYPE X |
| DATE EFFECTIVE: 04/01/1983 DATE PREPARED: 8/21/2009 | 604.29C |
| | SHEET NO. 1 OF 2 |

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

| BAR BILL - EXTENSION | | | | | | | |
|----------------------|------|-------|--------|-------|--------|-------|--------|
| LENGTH OF EXT. L | | 2'-6" | | 5'-0" | | 7'-6" | |
| MARK | SIZE | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH |
| G | #5 | 4 | 3'-3" | 7 | 3'-3" | 10 | 3'-3" |
| H ₁ | #4 | 4 | 2'-6" | 4 | 2'-6" | 4 | 2'-6" |
| H ₂ | #4 | 2 | 2'-9" | 3 | 2'-9" | 3 | 2'-9" |
| H ₃ | #4 | | | 2 | 3'-0" | 3 | 3'-0" |
| H ₄ | #4 | | | | | 2 | 3'-3" |
| H ₅ | #4 | 1 | 1'-0" | 1 | 1'-0" | 1 | 1'-0" |
| H ₆ | #4 | 2 | 1'-3" | 3 | 1'-3" | 3 | 1'-3" |
| H ₇ | #4 | | | 2 | 1'-6" | 3 | 1'-6" |
| H ₈ | #4 | | | | | 2 | 1'-9" |
| K | #4 | 7 | 1'-9" | 10 | 1'-9" | 13 | 1'-9" |
| L | #4 | 11 | 2'-9" | 11 | 5'-3" | 11 | 7'-9" |
| DOWEL BAR | #4 | 8 | 1'-0" | 9 | 1'-0" | 10 | 1'-0" |

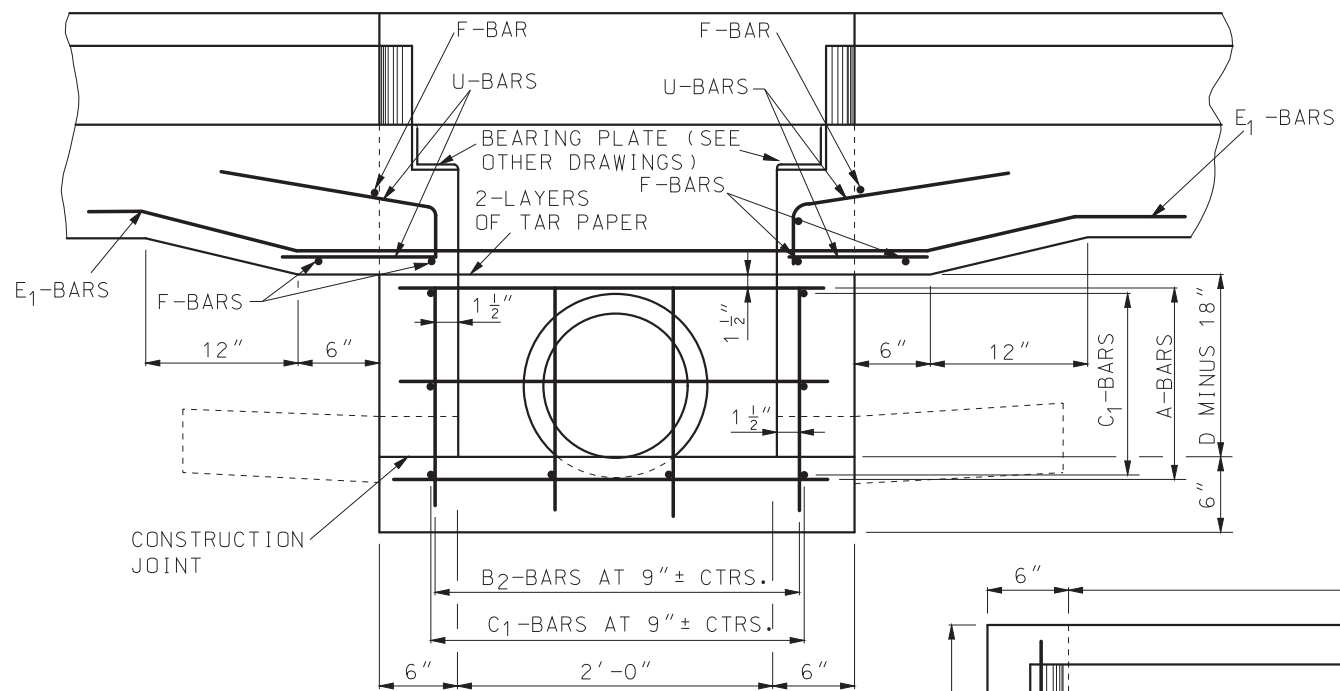
| CU. YDS. & LBS. ADDITIONS TO BE MADE FOR EACH EXTENSION | | |
|---|----------|-------|
| LENGTH | CU. YDS. | LBS. |
| 2'-6" | 0.39 | 60.0 |
| 5'-0" | 0.70 | 101.4 |
| 7'-6" | 1.04 | 143.8 |

| CU. YDS. DEDUCTION TO BE MADE FOR PIPE OPENING | | | |
|--|------|------|------|
| 12" | 15" | 18" | 24" |
| 0.03 | 0.04 | 0.05 | 0.09 |

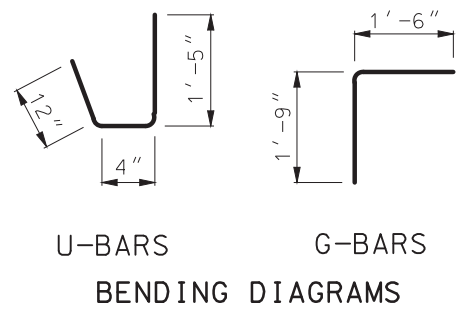
| CONCRETE AND STEEL DEDUCTIONS TO BE MADE FOR EACH EXTENSION OPENING | | |
|---|-----------|-----------|
| 9" X 12" | 12" X 12" | 15" X 12" |
| 0.01 C.Y. | 0.02 C.Y. | 0.02 C.Y. |
| 2.5 LBS. | 3.7 LBS. | 4.0 LBS. |

| BAR BILL - INTAKE BOX | | | | | | | | | | | | | | | | | | | | |
|-----------------------|----------------|------------|----------------------------------|--------|----------------------|--------|----------------------|--------|---------------------------------|--------|------------------------------|--------|------------------------|--------|------------------------|--------|----------------------|--------|-----------|--------|
| D | CONC. CU. YDS. | STEEL LBS. | A-BARS | | B ₁ -BARS | | B ₂ -BARS | | C ₁ -BARS | | C ₂ -BARS | | E ₁ -BARS | | E ₂ -BARS | | F-BARS | | U-BARS | |
| | | | TRANSVERSE IN BOTTOM WALLS & TOP | | VERTICAL IN WALLS | | VERTICAL IN WALLS | | LONGITUDINAL IN BOTTOM AND SIDE | | LONGITUDINAL IN SIDE AND TOP | | LONGITUDINAL IN GUTTER | | LONGITUDINAL IN GUTTER | | TRANSVERSE IN GUTTER | | IN GUTTER | |
| | | | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH | NO. | LENGTH |
| 2'-6" | 0.82 | 131 | 15 | 2'-9" | 8 | 2'-9" | 10 | 1'-3" | 8 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 2'-9" | 0.88 | 134 | 15 | 2'-9" | 8 | 3'-0" | 10 | 1'-6" | 8 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 3'-0" | 0.94 | 137 | 15 | 2'-9" | 8 | 3'-3" | 10 | 1'-9" | 8 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 3'-3" | 1.00 | 140 | 15 | 2'-9" | 8 | 3'-6" | 10 | 2'-0" | 8 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 3'-6" | 1.06 | 152 | 17 | 2'-9" | 8 | 3'-9" | 10 | 2'-3" | 10 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 3'-9" | 1.12 | 155 | 17 | 2'-9" | 8 | 4'-0" | 10 | 2'-6" | 10 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 4'-0" | 1.18 | 158 | 17 | 2'-9" | 8 | 4'-3" | 10 | 2'-9" | 10 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 4'-3" | 1.24 | 170 | 19 | 2'-9" | 8 | 4'-6" | 10 | 3'-0" | 12 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 4'-6" | 1.30 | 173 | 19 | 2'-9" | 8 | 4'-9" | 10 | 3'-3" | 12 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 4'-9" | 1.36 | 176 | 19 | 2'-9" | 8 | 5'-0" | 10 | 3'-6" | 12 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |
| 5'-0" | 1.42 | 189 | 21 | 2'-9" | 8 | 5'-3" | 10 | 3'-9" | 14 | 4'-3" | 6 | 1'-3" | 5 | 7'-0" | 1 | 4'-9" | 6 | 2'-9" | 8 | 2'-9" |

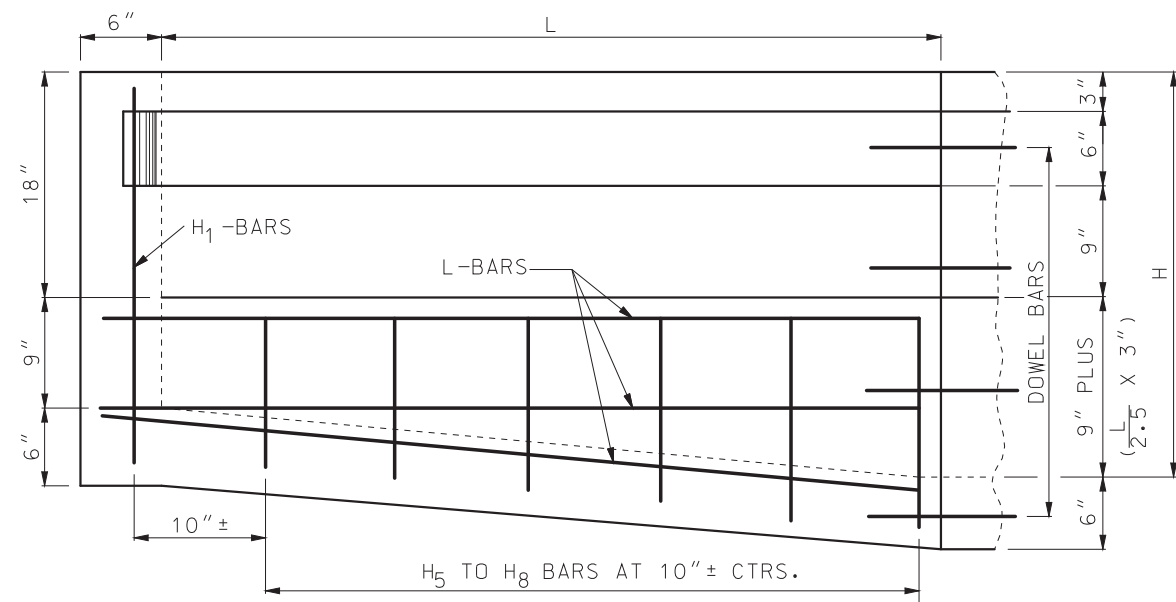
CONCRETE QUANTITIES INCLUDE 0.15 CU.YDS. FOR INVERT ALL BARS #4



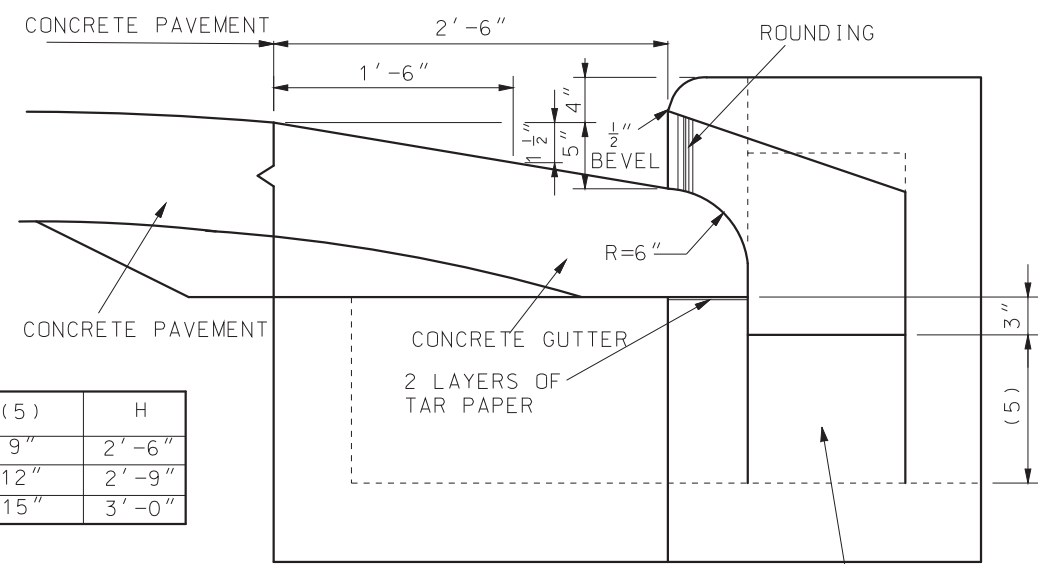
SECTION B-B



U-BARS G-BARS BENDING DIAGRAMS



LONGITUDINAL SECTION (EXTENSION)



SECTION SHOWING DETAILS OF OPENING AND DEPRESSION IN PAVEMENT OR GUTTER

| L | (5) | H |
|-------|-----|-------|
| 2'-6" | 9" | 2'-6" |
| 5'-0" | 12" | 2'-9" |
| 7'-6" | 15" | 3'-0" |

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
 105 WEST CAPITOL JEFFERSON CITY, MO 65102
 1-888-ASK-MODOT (1-888-275-6636)

MoDOT

STATE OF MISSOURI
 ERIC E. SCHROETER
 NUMBER PE-28411
 PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED SEALED AND DATED ELECTRONICALLY.

DROP INLET TYPE X

DATE EFFECTIVE: 04/01/1983
 DATE PREPARED: 2/9/2018

604.29C

SHEET NO. 2 OF 2

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.